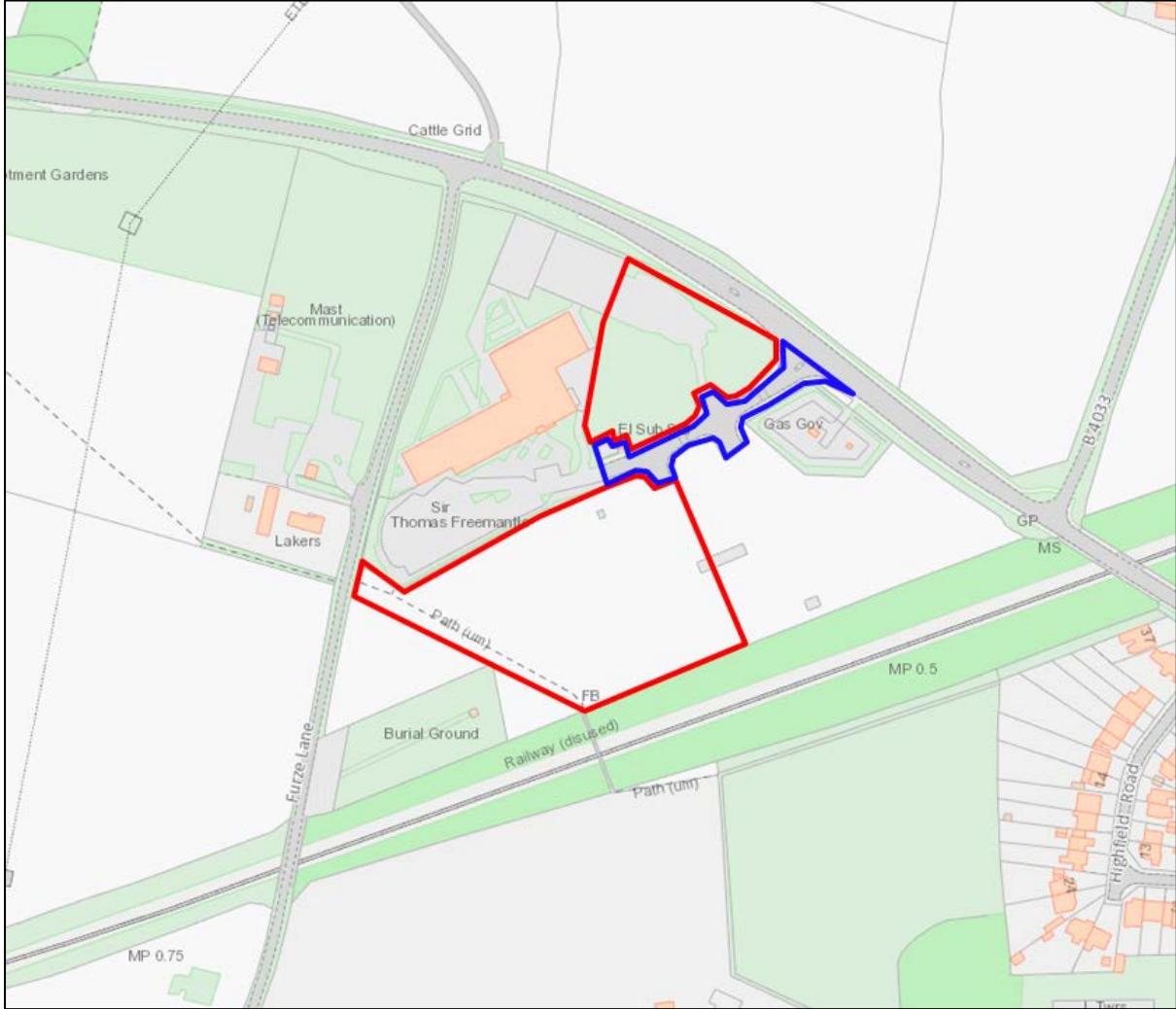


18/02599/ADP



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REFERENCE NO	PARISH/WARD	DATE RECEIVED
18/02599/ADP	WINSLOW	
<p>APPLICATION FOR APPROVAL OF RESERVED MATTERS PURSUANT TO OUTLINE PERMISSION 13/02112/AOP FOR B1(BUSINESS) B2 (GENERAL INDUSTRY) AND B8 (STORAGE AND DISTRIBUTION) USES WITH ANCILLARY OFFICE ACCOMMODATION, PROVISION OF RAIL STATION WITH ASSOCIATED PARKING, LANDSCAPING AND ACCESS</p> <p>LAND AT BUCKINGHAM ROAD, CREVICHON PROPERTIES LTD STREET ATLAS PAGE NO. 53</p>	<p>The Local Member(s) for this area is/are: - Councillor Llew Monger Councillor Susan Renshell</p>	23/07/2018

1.0 The Key Issues in determining this application are:-

- a) Principle of the development
- b) Access
- c) Layout
- d) Scale
- e) Appearance
- f) Landscaping
- g) Other material planning considerations

The recommendation is that the reserved matters be APPROVED subject to conditions.

2. CONCLUSION AND RECOMMENDATION

- 2.1 This detailed scheme follows the grant of permission of the outline proposal 13/02112/AOP where the principle of the development for the wider employment development was accepted and this is a material consideration in the determination of this application.

- 2.2 The application has been evaluated against the Development Plan and the NPPF. Paragraph 11 of the NPPF sets out the presumption in favour of sustainable development which for decision taking this means approving development proposals that accord with an up-to-date development plan without delay; or where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date, granting permission unless the application of policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.
- 2.3 The Winslow Neighbourhood Plan 2014-2031 (WNP) and AVDLP contain relevant policies including policy 6 of the WNP and GP8, GP24, GP35, GP38-40 of AVDLP which are the most important and up to date policies therefore paragraph 11d) of the NPPF is not engaged.
- 2.4 A previous application relating to the reserved matters for the construction of the business units at this site was granted (ref: 18/02598/ADP – approved in February 2019). Amendments to the development are sought in order to allow some flexibility to the type of unit brought to the market and to potential future occupiers of the site. As with the previous application, it is considered the proposal would positively contribute to the economic, social and environmental conditions of the area. r and appearance of the site, street scene or landscape character of the area.
- 2.5 Compliance with some of the other objectives of the NPPF have been demonstrated in terms of the highway impact and parking provision, promoting healthy communities, the design of the development, impacts on the natural environment, flood risk and on residential amenity. However, these matters do not represent benefits to the wider area but demonstrate an absence of harm to which weight should be attributed neutrally.
- 2.6 It is considered that the proposal accords with the most important policies in the WNP and AVDLP and there are no material considerations that indicate a decision other WNP and AVDLP and there are no material considerations that indicate a decision other than in accordance with those policies.
- 2.7 It is therefore recommended that the application be **GRANTED** subject to the following conditions:
- .
1. The use of the building at Plot A hereby approved (or part thereof) shall be for B8 (storage and distribution) with a floor space of no greater than 2003sq.m, including ancillary office accommodation, and at no time shall the building be used for any purposes within B1 (offices and light industrial) or B2 (general industrial) use classes

as defined by the Town and Country Planning (Use Classes) Order 1987 (as amended).

Reason: To ensure the building benefits from provision of off-road car parking, turning and large vehicle operating areas appropriate to its use in accordance with Policy 6 of the Winslow Neighbourhood Plan 2014-2031, saved policies GP.24 and WI.2 of the Aylesbury Vale District Local Plan, Appendix 2 (Parking Standards) of the emerging Vale of Aylesbury Local Plan and the relevant provisions of the National Planning Policy Framework.

2. The use of the building at Plot B hereby approved (or part thereof) shall be for B2 (general industrial) or B8 (storage and distribution) with a floor space not exceeding 4755sq.m, including ancillary office accommodation, and at no time shall the building be used for any purposes within B1 (offices and light industrial) use classes as defined by the Town and Country Planning (Use Classes) Order 1987 (as amended).

Reason: To ensure the building benefits from provision of off-road car parking, turning and large vehicle operating areas appropriate to its use in accordance with Policy 6 of the Winslow Neighbourhood Plan 2014-2031, saved policies GP.24 and WI.2 of the Aylesbury Vale District Local Plan, Appendix 2 (Parking Standards) of the emerging Vale of Aylesbury Local Plan and the relevant provisions of the National Planning Policy Framework.

Informative(s):

1. You are advised that, unless expressly indicated, nothing within this approval effects or varies the conditions imposed on outline planning permission no. 13/02112/AOP dated 29 November 2013 which must be complied with.
2. In accordance with paragraphs 39 and 39 of the National Planning Policy Framework, the Council, in dealing with this application, has worked in a positive and proactive way with the Applicant / Agent and has focused on seeking solutions to the issues arising from the development proposal.

AVDC works with applicants/agents in a positive and proactive manner by:

- offering a pre-application advice service;
- updating applicants/agents of any issues that may arise in the processing of their application as appropriate and, where possible and appropriate, suggesting solutions.

In this case, AVDC has considered the details of the development as submitted and amended information which were considered acceptable.

3. INTRODUCTION

- 3.1. The application is referred to planning committee as the parish council has raised material planning objections with regards to the visual impact of the development and intend to speak at committee.

4. SITE LOCATION AND DESCRIPTION

- 4.1. The site is located within the northern fringe of Winslow and comprises part of a wider. 4.35 Ha site with the new Freemantle school occupying the north-western area of the land. There is a gas distribution facility adjoining the site on the eastern boundary shared with the public highway (A413 - Buckingham Road) and, there is also a small area in the south-western corner that is reserved for a future extension of the adjoining burial ground. A public right of way (PROW) runs across part of the south-western area (numbered WIS/6/1 by Bucks CC).
- 4.2. The recently constructed access road which serves the school also provides access to the site separating the two parcels, to the north east and south west of the site, from Buckingham Road.

5. PROPOSAL

- 5.1. This is a reserved matters application seeking appearance, landscaping and layout. The current proposal is for the construction of a two-storey warehouse/production unit providing approximately 1900sq.m at Plot A and for a two-storey warehouse/production unit providing 4600sq.m of internal floor space at Plot B. The current scheme would provide only the two larger units. This is an alternative scheme to that granted approval under 18/02598/ADP in February 2019 and is sought to provide some flexibility in terms of the type of buildings that could be brought to the market depending on the requirements of prospective tenants.
- 5.2. The building at Plot A would be set close to the north-western plot boundary shared with the neighbouring school. 16 off-road car parking spaces (with dedicated disabled, cycle and motorbike parking) would be located to the east of the plot with a large gated service yard to the south-west. The building would have a height of approximately 11.3 metres to the ridge with eaves heights of approximately 8.5 metres at the south-eastern side elevation and 9.25 metres at the north-western side elevation. The most visible elevation within public views would be the north-east facing rear elevation (facing towards Buckingham Road). It would have a width of approximately 48 metres. The building would be finished with grey profile roof cladding with walls a mixture of anthracite horizontal cladding and metallic silver horizontal profile cladding and anthracite coloured aluminium framed glazed sections, windows, doors and rainwater goods.
- 5.3. The building at Plot B would be set close to the south-eastern plot boundary shared with the adjacent public footpath, existing burial ground and proposed extension to the burial ground. Approximately 100 off-road car parking spaces (with 5% dedicated disabled spaces) would be located to the north and east of the plot with a large gated

service yard to the south, abutting the railway line boundary. The building would have a height of approximately 13.2 metres to the eaves and approximately 15 metres to the ridge. The most visible elevation within public views would be the south-west facing side elevation (facing towards the public footpath that runs between Furze Lane and the pedestrian railway bridge) having a width of approximately 49 metres. The public footpath would be set approximately 3.5 metres from the southern corner of the building. The north-west facing front elevation (facing the school) would have a width of approximately 82 metres.

- 5.4. Both buildings would be finished with grey profile roof cladding with walls a mixture of anthracite (dark grey) horizontal cladding and metallic silver horizontal profile cladding. Anthracite coloured aluminium framed glazed sections, windows, doors and rainwater goods are also proposed. Hard-surfacing would be comprised from a mixture of tarmac site roads, permeable block paving (with below ground surface water attenuation tanks) for the car parking areas, block paving for pedestrian footways and concrete service yards.
- 5.5. Landscaping for both plots would involve tree, hedge and shrub planting predominantly along the site perimeters. A small coppice of native Field Maple trees would be planted at the southern point of Plot B to provide some vegetative screening between the building and burial ground to the south-west. A native hedge mix would be planted along the site boundary shared with the public footpath and the north-western site boundary shared with the school. A hornbeam hedge would be planted between Plot B and the new railway station site. A scheme of landscaping for the estate road has already been undertaken on the land between Plot A and the public highway at Buckingham Road. This would be enhanced by a mixed-native hedge planted along the north-eastern boundary and a coppice of Field Maple planted between the new building and adjacent school site. A row of Whitebeam 'Lutescens' would be planted along the eastern edge of the plot shared with the estate road and main access from the public highway.

6. RELEVANT PLANNING HISTORY

- 13/02112/AOP - Outline application for B1(Business) B2 (General Industry) and B8 (Storage and Distribution) Uses with ancillary office accommodation, provision of rail station with associated parking , landscaping and access – Granted subject to conditions.
- 13/A2112/DIS - Submission of details pursuant to Conditions 5 (landscaping), 9 (archaeology), 10 (surface water drainage), 11 (contaminated land assessment), 14 (estate roads), 15 (access junction works), 16 (bus stop), 20 (ecology) and 21 (construction compound & temporary access) relating to outline permission 13/02112/AOP – Details approved.
- 16/03132/ADP - Approval of reserved matters pursuant to outline permission 13/02112/AOP relating to access, appearance, landscaping, layout and scale for the provision of the rail station building, associated parking and internal road. – Approved subject to conditions.

- 18/02598/ADP - Application for Approval of Reserved Matters pursuant to outline permission 13/02112/AOP relating to appearance, landscaping, scale and layout for B1(Business) B2 (General Industry) and B8 (Storage and Distribution) Uses with ancillary office accommodation, provision of rail station with associated parking , landscaping and access – Approved subject to conditions – February 2019.
- 18/04244/ADP - Reserved Matters Application pursuant to outline permission 13/02112/AOP relating to access, appearance, landscaping, layout and scale for the provision of the rail station building, associated parking and internal road – Pending consideration.
- 19/02394/APP - Removal of condition 4 (removal of operational hours) relating to application 18/02598/ADP – Pending consideration.

7. PARISH/TOWN COUNCIL COMMENTS

7.1 Winslow Parish Council - **object** to the proposals.

The building at Plot A would be overly dominant in its setting and, for such a large building, is situated too close to the A413.

The building at Plot B may also conflict with Network Rail's Transport and Works Act Order application for East West Rail, and also with the subterranean high-pressure gas main.

WTC wishes to speak at any committee meeting determining this application.

8. CONSULTATION RESPONSES

- 8.1 Buckingham County Council Highways – No objection subject to conditions.
- 8.2 Initial comments were made in August 2018 which confirms that the parking spaces provided are of adequate dimensions and benefit from an adequate level of manoeuvrability and raising concerns over the large vehicles would struggle to access the turning areas due to the proposed layout and the proximity to the roundabout to lot A and tracking for plot B. Mindful of this, a swept path analysis showing refuse vehicle and articulated vehicles entering, manoeuvring through the site, and exiting is required to demonstrate that these manoeuvres are possible.
- 8.3 Due to the proximity of the junction into the car park to the main access into the site, there are concerns over the potential for sudden and unexpected braking manoeuvres and for vehicles to be backed up onto the main access into the site.
- 8.4 Further consideration needs to be given to pedestrian access; the area to the south of the disabled spaces should be widened to 2m to ensure that all pedestrians are able to access all areas of the site.

8.5 Mindful of the above, there are concerns over the manoeuvring associated with both plots, particularly Plot A, and the additional information discussed above is required.

8.6 Following the submission of additional information and some discussion between BCC and the applicant BCC are now satisfied that both aspects of the scheme are acceptable, and no objections are made subject to conditions to secure provision of the parking and manoeuvring areas and suitable signage is erected at the entrance to Plot B in accordance with the approved plans.

8.7 Environmental Health – No comments to make.

As at this stage there are no details of the future occupancy of the units there are no environmental health comments for this application.

8.8 Network Rail – No objection.

8.9 Comments were submitted with regards to reserved matters scheme 19/02598/ADP (for an alternative scheme at the current application site). No objection was made but the following comments were submitted:

8.10 *The proposal shows the old layout of the station site. Updated details were included in the station planning application submitted Q4 2018.*

- *The proposal shows a line of specimen trees planted along the perimeter of the station decked car park. This is likely to be impractical, undesirable from a maintenance perspective and unsustainable in the long term.*
- *The proposal shows industrial units adjacent to the boundary with 'Potential CPO Land'. Discussions with EWR2 has agreed a smaller part of this plot to be used for construction access by EWR2, in exchange for construction access along the north and west perimeter of this planning application. This construction access route is within the ownership of the applicant.*
- *The proposal may conflict with the SGN high pressure gas main corridor, along the north edge of plot B.*

8.11 Comments were sought from Network Rail for the current scheme but, none were received.

8.12 Archaeology – No comments to make.

8.13 River Ouzel Drainage Board – No comments to make.

9. REPRESENTATIONS

9.1 The application was the subject of press and site publicity. No responses were received.

10. EVALUATION

10.1 The overview report attached sets out the background information to the policy framework when making a decision on this application.

10.2 Winslow Neighbourhood Development Plan made 2014-2031

- 10.3** Policy 1 relates to the presumption in favour of Sustainable Development. Policy 6 relates to Land South of Buckingham Road, it allocates the site for a mixed use scheme comprising B1, B2 and B8 units (1.5 ha), a railway station and associated links (1.35ha) and education use (1.5ha). Policy 6 provides parameters for the redevelopment of the site by dividing the site into 3 areas to deliver a station, school and employment units.
- 10.4 Policy 6 requires the preparation of a masterplan to ensure proper planning and delivery processes are followed.

10.5 Aylesbury Vale District Local Plan

- 10.6** A number of general policies of the AVDLP are considered to be consistent with the NPPF and therefore up to date so full weight should be given to them. Consideration therefore needs to be given to whether the proposal is in accordance with or contrary to these policies. Those of relevance are WI2 , GP8, GP24, GP35, GP38 – GP40 and GP45..

10.7 Vale of Aylesbury Local Plan 2013-2033

- 10.8** The overview reports sets out the latest position with regards to VALP. The most relevant policies are T5 - Delivering transport in new development; T6 - Vehicle Parking; T7 - Footpaths and cycle routes; T8 - Electric Vehicle Parking; BE2 - Design of new development; NE1 – Biodiversity and geodiversity; NE4 – Landscape character and locally important landscape; NE8 – Trees, hedgerows and woodlands; C4 – Protection of public rights of way; I1 – Green infrastructure.
- 10.9 The weight to which individual policies relate to the material planning considerations of this application will be undertaken within the more detailed assessments in the report sections below.

10.10 The principle of the development:

- 10.11 Outline permission was granted for the employment units as part of the wider mixed use outline consent with means of access considered under 13/02112/AOP . As such, the principle of development on this site has been previously established and accepted. The matters reserved for subsequent consideration are assessed below.
- 10.12 The outline planning permission was granted subject to a number of conditions, including a condition ensuring subsequent reserved matters details were in compliance with a parameters plan that set limits to the uses to be permitted at the site and limited heights of new buildings to not exceed 15 metres. An indicative site layout also outlined expected overall floor-spaces of new buildings to be provided. Matters relating to the site access and layout of the internal site road were approved in April 2016 and the estate road and site access constructed thereafter. The current application only seeks approval of matters relating to appearance, landscaping, layout and scale for the employment units to be constructed at Plots A and B so the

principle of the development is not required to be reassessed. An alternative set of reserved matters details have also already been approved.

10.13 Reserved Matter: Layout:

10.14 The outline permission included an indicative layout / masterplan identifying development zones.

10.15 NP Policy 6 states that a masterplan for the entirety of the site should be prepared. This does not appear to have been the case but, several points required to be addressed through the masterplan can be addressed against the layout of the reserved matters. These are:

- *it can demonstrate that a single access on to Buckingham Road to serve all the proposed land uses can be satisfactorily achieved;*
- *provision is made for the retention of the public right of way over the land and over the railway line to Land East of Furze Lane;*
- *a travel plan demonstrates how the station operator will manage car parking by rail passengers to avoid the use of existing public car parks or residential streets in the town; and*
- *a satisfactory layout, design and landscape strategy can be achieved that will protect the amenities of each land use.*

10.16 Policy GP.35 of the AVDLP requires new development to respect and complement the physical characteristics of the site and surroundings; the building tradition, ordering, form and materials of the locality; the historic scale and context of the setting; the natural qualities and features of the area; and the effect on important public views and skylines. This policy is considered to be consistent with the NPPF. VALP emerging policy BE3 is consistent with GP35..

10.17 Policy GP.38 states that development schemes should include landscaping proposals designed to help buildings fit in with and complement their surroundings, and conserve existing natural and other features of value as far as possible. VALP policy NE8 is consistent with GP38-39.

10.18 Policy GP.84 states that development affecting a public right of way the Council will have regard to the convenience, amenity and public enjoyment of the route and the desirability of its retention or improvement for users, including people with disabilities. Planning conditions will be imposed on planning permissions, or planning obligations sought, to enhance public rights of way retained within development schemes.

10.19 The impact of the development of the site as a whole on the wider landscape has already been assessed and found acceptable as part of the consideration of the outline permission.

- 10.20 Condition 4 of the outline planning permission sought to ensure the development did not exceed the parameter plans established by the outline planning permission. Approved plan no. 002 – Parameters Plan with Land Use Zones set out areas where B1, B2 and B8 uses were permitted and limited building heights to not exceed 15 metres. Plot B would have a ridge height of 15 metres with Plot A would have a ridge height of 11.3 metres. Both buildings would be constructed within the site area permitted to be used for employment use(s). Therefore, the details submitted are in line with the parameters of the outline planning permission.
- 10.21 Vehicular access was approved at the outline stage in the form of a single access point off the Buckingham Road and the reserved matters of the main spine road were subsequently approved under application 16/03132/ADP. The proposed employment units would continue to be served by the approved access arrangements and are located in the two remaining parcels of development land. The warehouse/production building is sited at the front of plot A, behind a landscape area addressing Buckingham Road with the entrance facing the internal access road and proposed station. Parking is located to the side and loading/unloading to the south west of the building with landscaping along the frontage, boundaries and access road which is secured by a condition of reserved matters application 16/03132/ADP.
- 10.22 The planting scheme proposed within the plot would enhance the existing scheme and, whilst the building would be visible from Buckingham Road, it is considered the proposed landscaping along the frontage would help to soften the impact and would also contribute to the street scene along Buckingham Road (increasingly so as trees and hedgerows mature).
- 10.23 The building at Plot B would be visible from the public footpath to the west. A native hedge mix would be planted along the south-eastern site boundary providing a soft screen to the footpath. A small coppice of Field Maples would be planted at the south-western point to soften views of the building from the adjacent burial ground. Within the plots hard-surfacing materials would be common for the type of development but, would be laid out with some variety and broken up with green areas of shrub planting.
- 10.24 Plot B involves a single employment units with parking to the north and west side and loading/unloading to the south . This is a typical layout in small industrial estates and in view of the limited size of the units appears to give a practical layout for plot B.
- 10.25 On this basis, the proposed layout addresses the site constraints and will deliver a development where the layout respects the requirements of the outline consent and, the adjacent land uses.
- 10.26 *Highway impact and parking:*
- 10.27 The proposed employment units would continue to be served by the approved access arrangements and are located in the two remaining parcels of development land. BCC are satisfied with the layout proposed in highway safety and convenience terms.

- 10.28 The Council's standards contained in SPG1 would require a maximum of spaces to be provided for the residential development. The layout of Plot A would provide 19(no) parking spaces including 1(no) disabled space. Two of the car parking spaces would permit electric vehicle charging and in addition dedicated motorbike and cycle parking. The emerging VALP Appendix 2 sets out optimum parking standards for various types of land use. VALP policies T6 and T8 can only be given limited weight due to the main modifications proposed.
- 10.29 For B8 (storage and distribution) a floor space of 2000sq.m would require 18 parking spaces as well as spaces for parking and turning of operational vehicles (HGVs). Plot A can accommodate the parking demand for a B8 use but, B2 (general industrial) would require 36 off-road parking spaces. Plot B would have room within the wider site for 91 off-road parking spaces (5 being accessible spaces). There would be 6 EV charging spaces, 16 covered cycle spaces and 2 motorcycle spaces. It would have a floor space of 4750sq.m so a B8 use would require 43 parking spaces, and a B2 (general industrial) would require 86 spaces.
- 10.30 Both plots would be able to accommodate sufficiently sized operational areas. Initially BCC Highways sought clarification that a 16.5 heavy goods vehicle could access and turn within the rear service yards. Additional information was submitted to confirm this would be the case and no further highway objection was made. In terms of off-road parking provision for employees and visitors Plot B would be able to accommodate enough off-road parking to serve B8 or B2 uses. Plot A would only be able to provide a sufficient amount of parking for a B8 use only. Neither plot would be able to accommodate a B1 use in terms of parking.
- 10.31 In view of the inability to provide the requirements for a B1 use it is considered that a restrictive use condition for Plot A is reasonable and necessary to ensure sufficient off-road parking and turning areas are secured. The estate road and public highway at Buckingham Road provide no acceptable on-road parking opportunity so there is not justification for sub-optimum off-road parking provision in my view. A condition for Plot B preventing a B1(a) and B1 uses would also be reasonable as 143 off-road parking spaces would be required for a B1 should the unit convert into such use(s) in the future under permitted development rights. The restrictive conditions would be linked to both the scale of the buildings and layout of the plots so are reasonably within the scope of the reserved matters in my view. A condition to require construction of all of the off-road parking, turning and servicing areas prior to the first use of each building is also reasonable and necessary.
- 10.32 Also relevant to the site layout is the connectivity between sites and provision of safe access for both vehicles and pedestrians. Some initial concerns were raised by BCC Highways due to the insufficient widths of several footpaths within the plots. Revised plans increased the widths of internal site footpaths to 2 metres to enable access for all users. An initial request was also made to the applicant to explore the option of running a public footpath through Plot B to provide a more direct link between the public footpath running to the rear of the burial ground and the new railway station. For public safety and operational reasons it is not feasible to connect the public footpath through Plot B to the railway station site. A public footpath link to the railway

station would be safeguarded, via the 'grass path' that runs to the north of Plot B across the intervening land between the school site.

10.33 Subject to the recommended conditions it is considered that the development would benefit from sufficiently sized parking, turning and operational areas and would not give rise to unacceptable harm to the safety or operation of the public highway in the locality in accordance with the NPPF. The proposed level of parking is in line with the AVDLP policy and parking SPG.

10.34 Making Effective Use of Land:

10.35 This development proposes a site coverage which would make effective use of the land in terms of the amount and the impact on the character and setting is assessed below.

10.36 Residential amenity:

10.37 The only residential properties within the vicinity of the site are those currently being constructed to the south beyond the disused railway line. Insofar as the reserved matters relate to appearance, landscaping layout and scale, there is little potential for harm to be caused to the amenities of the nearest residential properties. The landscaping would mitigate the most available public views. In terms of layout the building at Plot B would be closest to neighbouring dwellings, being the bungalow at Lakers Nursery (approximately 85 metres to the north-west) and the new housing development (approximately 75 metres to the south, beyond the railway).

10.38 The previously approved reserved matters details included a condition restricting operating hours of the industrial units permitted. The applicant has applied for removal of the restrictive hours condition (19/02394/APP) and Environment Health have raised no objection to the removal of this condition (so there is a future possibility of the application being approved). The outline planning permission also has a condition, linked to the reserved matters for the railway station, that will require future submission of a noise survey and attenuation strategy for the site, including impacts that may arise from the employment units. This would allow some further control over noise issues if it was found that excessive noise would be experienced by neighbouring occupiers. Overall, it is considered that the reserved matters details would not give rise to harm to neighbouring residential amenities or more generally the amenities of the area in accordance with NP Policy 6, policies GP.8 and GP.35 of the AVDLP, emerging policy BE2 of the VALP and paragraph 127(f) of the NPPF

10.39 On this basis the proposed layout addresses the site constraints and will deliver a development where the layout respects the requirements of the outline consent and, the adjacent land uses, subject to conditions as specified.

10.40 Reserved Matters: Scale and Appearance:

10.41 Having regard to the separation from the main built-up area of the town, that extends to the south of the railway, and the recent construction of the large modern school there is little in the way of distinctive characteristics or features of the area.. There are also certain constraints that would arise from the type of land-use permitted and

the expected needs of the market (as the buildings will be for commercial use). Aside from the new school and estate road the site remains as undeveloped land, mostly gone to scrub. Much of the land surrounding the site is open farmland with few overly visible buildings.

- 10.42 The school is a large modern building possessing the distinctive character of a modern educational establishment. Its southern front elevation has an interesting mix of forms and modern materials with a palette utilising the school colours as well as more neutral greys and blacks. The least interesting elevation of the school (the indoor sports hall) extends east, towards the western boundary of Plot A. The sports hall wing of the school is a steel frame two-storey building clad in grey metal sheeting.
- 10.43 The two employment units would be of a similar overall scale to the school but, of simpler designs (as befitting their function). They would be clearly visible within the surrounding area, particularly so from Buckingham Road to the east and the footpath skirting the south-western boundary. However, they would be of a form and design in character with the land-use permitted and are of an appropriate scale and simple appearance. The modern external materials proposed would reflect that of the school building but, without competing with its more interesting form. Some variation to the external materials and use of areas of floor-to-ceiling glazing (serving the ancillary offices) would be proposed to add some interest to the long, mostly blank elevations.
- 10.44 Whilst the WTC has raised concerns over the building on plot A being overly dominant and too close to the A413, it is considered that the building is set back from the road frontage similar to that on the already approved reserved matters scheme for a 3 storey office building and appropriate landscaping would reduce the impact in wider views.
- 10.45 It is considered that the scale and appearance of the buildings is acceptable and meets the requirements of Policy 6 of the Winslow Neighbourhood Plan and GP35 of the Local Plan and the NPPF.

10.46 *Reserved Matter: Landscaping:*

- 10.47 As there is less scope to seek highly architecturally interesting building designs (due to the nature of the land use permitted) as set out above, the landscaping of the plots will be important, to ensure some green screening is achieved. A site-planting scheme is proposed that would introduce some areas of tree planting, native hedge lined boundaries and smaller areas of shrub and bulb planting with the sites. The areas of tree planting appear to have been judiciously considered to give the most screening within public views (from the burial ground to the south-west and Buckingham Road to the east).
- 10.48 With regard to Plot A, the estate road development already involved some areas of tree and shrub planting that have been undertaken and appear established within the land between Plot A and the public highway. The planting scheme proposed within the plot would enhance the existing scheme and, whilst some of the building would be visible, it is considered that there would be a good level of greenery created

within the site that would also contribute to the street scene along Buckingham Road (increasingly so as trees and hedgerows mature).

- 10.49 Similarly, the building at Plot B would be visible from the public footpath linking the new housing development (to the south of the railway bridge) to Furze Lane and the burial ground. A native hedge mix would be planted along the south-eastern site boundary providing a soft screen to the footpath. A small coppice of Field Maples would be planted at the south-western point to soften views of the building from the adjacent burial ground. Within the plots hard-surfacing materials would be common for the type of development but, would be laid out with some variety and broken up with green areas of shrub planting.
- 10.50 On balance, it is considered that the appearance, landscaping, layout and scale of the buildings is acceptable and the landscaping scheme would offer good levels of greenery within each plot with new tree planting located in key areas to soften the most affected public views. A planning condition can be used to secure the full planting scheme is completed prior to the first use of the building. As such, it is considered there would be no harm arising from the scheme and no conflict with the aims of NP Policy 6, policies, GP.35 and GP.38 of the AVDLP, paragraph 127 of the NPPF or emerging policy BE2 of the VALP.

10.51 *Other material planning considerations:*

10.52 Building a strong, competitive economy:

- 10.53 The Government is committed to securing and supporting sustainable economic growth in order to create jobs and prosperity by taking a positive approach to sustainable new development. It is considered that there would be economic benefits in terms of the construction and operation of the development itself and the resultant increase in population contributing to the local economy which, should be afforded considerable weight in the scheme's favour.

10.54 Conserving and enhancing the natural environment:

- 10.55 The site is currently mostly undeveloped land (aside from the school). Historically it appears to have been an open field, possibly used for pasture, but it has been allocated for development for a substantial number of years. It appears to have had tree and hedge line boundaries, a large section of which was removed to construct the access from Buckingham Road. The construction of the buildings themselves is unlikely to impact directly on protected species or valuable habitat. However, the development has resulted in some loss of established hedgerow and trees so it is reasonable to expect a net loss of biodiversity and green infrastructure across the site. The introduction of additional hard-surfaced areas would further cause harm to the biodiversity value of the site.
- 10.58 Several long stretches of native hedge would be planted, along with various shrubs beds and small groups of native trees. The site planting scheme would restore some of the native hedges and trees lost to the development. It would also provide several

wildlife corridors through the site, linking the town to the south, the railway line, the site and open countryside to the north and west. Whilst not strictly within the scope of the reserved matters sought for approval, I consider the biodiversity and green infrastructure benefits that would arise from the site landscaping scheme are a merit of the scheme and, on balance, the reserved matters do not conflict with the aims of NPPF paragraph 170 of the NPPF and emerging policy NE1 of the VALP (given less weight to the NPPF due to the modifications currently proposed to it).

10.59 Meeting the challenge of climate change and flooding:

10.60 The NPPF at Section 14, 'Meeting the challenge of climate change, flooding and coastal change' advises at paragraph 163 that planning authorities should require planning applications for development in areas at risk of flooding to include a site-specific flood risk assessment to ensure that flood risk is not increased elsewhere, and to ensure that the development is appropriately flood resilient, including safe access and escape routes where required, and that any residual risk can be safely managed. Development should also give priority to the use of sustainable drainage systems.

10.61 Flooding and drainage matters were assessed as part of the outline permission which has a condition requiring the submission of a drainage scheme before works commence in accordance with the NPPF

10.62 Other matters raised by the WTC:

10.63 Matters relating to the potential impacts on the high-pressure gas pipeline were considered at outline permission stage and the applicant has had ongoing discussions with the operator of the pipeline to ensure no harm would be caused to it.

10.64 Similarly, the applicant has confirmed discussions with Network Rail are also ongoing with regards to site ownership and access to permit construction of the new station. However, these discussions and any future agreements are mainly civil matters to be decided between the adjacent landowners. The railway site has been safeguarded in line with the relevant planning policy and the site is now close to the point of delivery. In planning terms, the reserved matters currently proposed do not appear to present any significant obstruction to the construction of the railway station or reopening of the East-West Rail line and no further planning control would be reasonable at this stage.

Case officer: David Millinship (dmillinship@aylesburyvaldc.gov.uk)